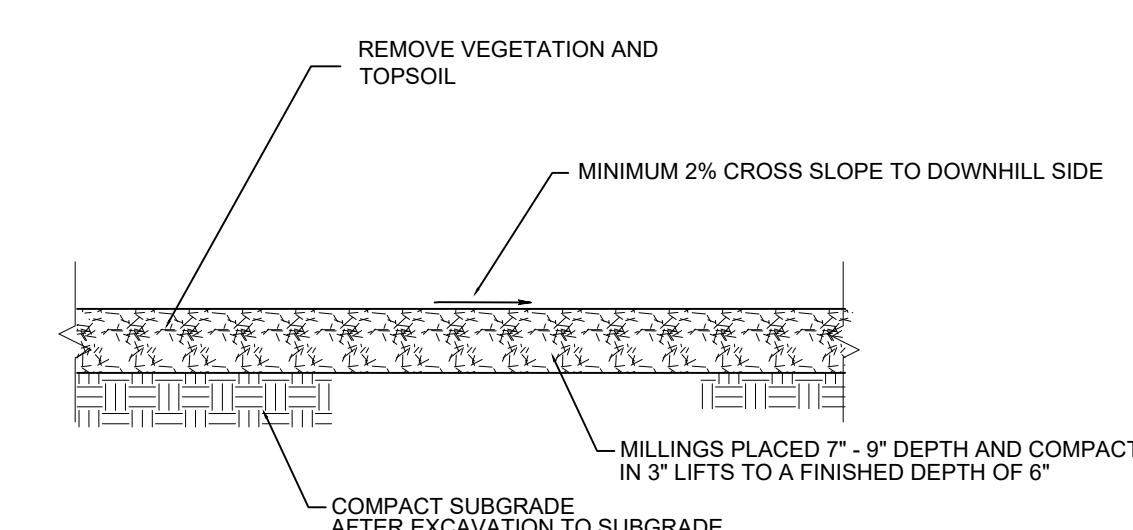
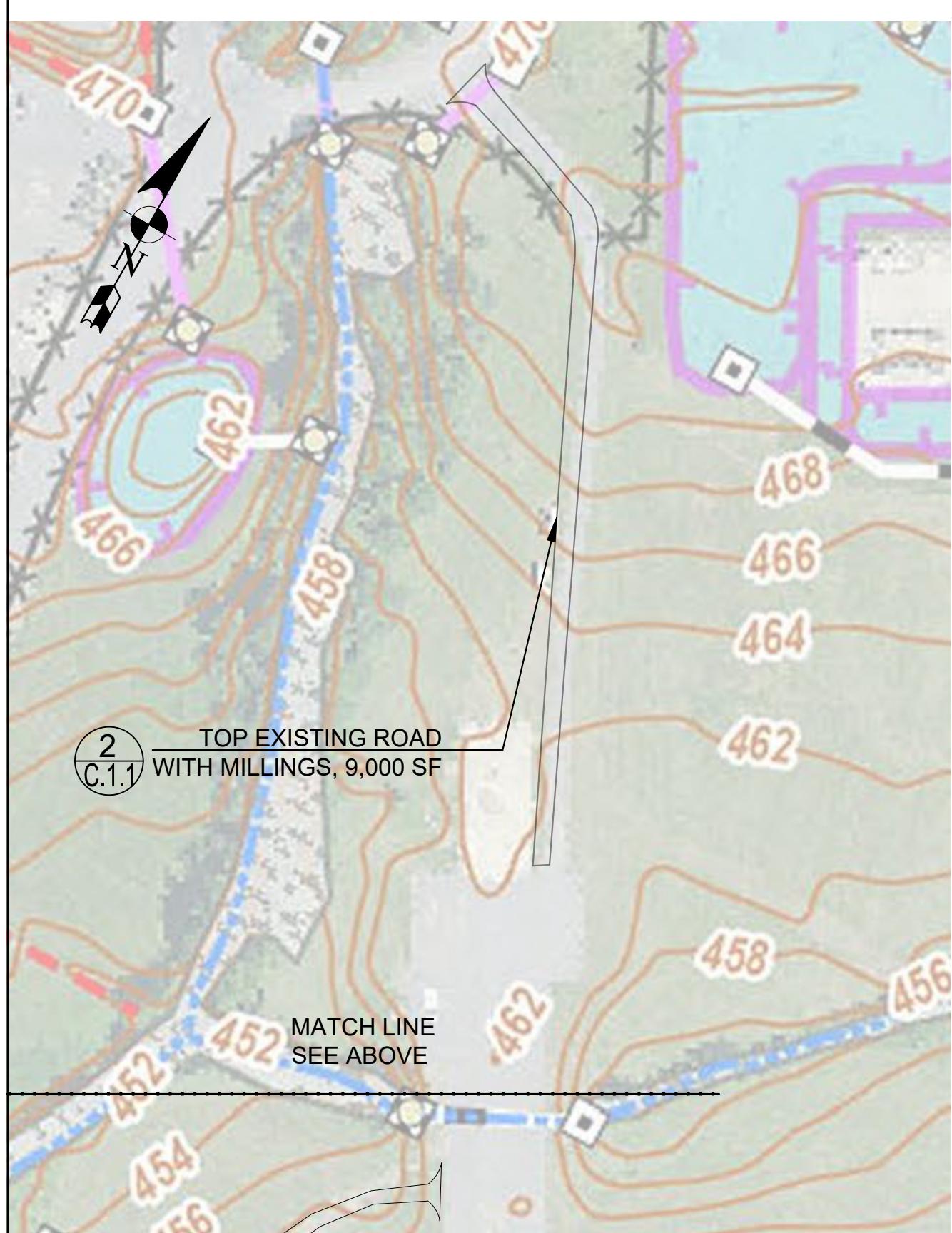


MUIR AIRFIELD ROADWAYS

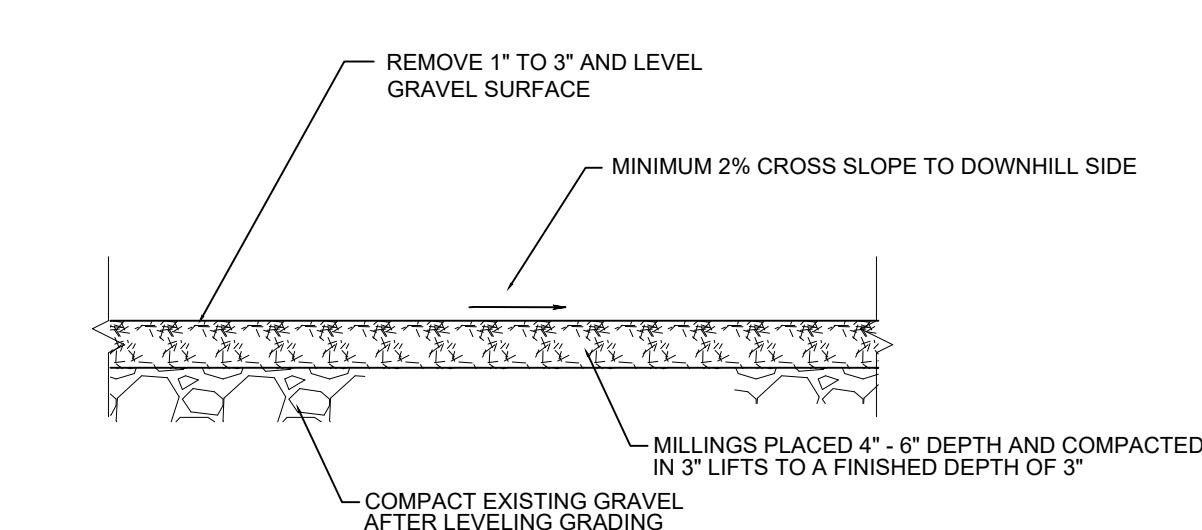
SCALE: 1" = 100'
SCALE IN FEET 100' 50' 0' 100' 200'

MILLING NOTES

1. MILLINGS SHALL BE FINE GROUND TO UNIFORM DIAMETER TO BE USED FOR PAVING NEW ROAD.
2. REMOVE LOOSE ASPHALT, DEBRIS, VEGETATION, AND TOPSOIL IN AREAS OF PROPOSED MILLINGS SURFACE TO A DEPTH OF 4-6 INCHES. DISPOSE OF LOOSE ASPHALT AND DEBRIS IN DUMPSTER AND VEGETATION AND TOPSOIL IN DESIGNATED AREAS.
3. GRADE AND COMPACT SURFACE FOR DRAINAGE AND SOLID BASE.
4. FOR NEW ROADS, SPREAD MILLINGS EVENLY TO A LOOSE DEPTH OF 7-9 INCHES. ROLL TO COMPACT AFTER EVERY THREE INCHES TO 6 INCHES AFTER COMPACTION.
5. FOR TOPPING EXISTING ROADS, SPREAD MILLINGS EVENLY TO A LOOSE DEPTH OF 4-5 INCHES. ROLL TO COMPACT TO 3 INCHES AFTER COMPACTION.



1 C.1.1 MILLINGS PAVEMENT DETAIL
NOT SCALE



2 C.1.1 MILLINGS TOPPING DETAIL
NOT SCALE

NO.	DESCRIPTION	DATE
	REVISIONS	

Professional's Signature Date

COMMONWEALTH OF PENNSYLVANIA
DEPT. OF MILITARY & VETERANS' AFFAIRS
ANNVILLE, PENNSYLVANIA 17003

DESIGN PROFESSIONALS:
OFFICE OF FACILITIES AND ENGINEERING
BUREAU OF DESIGN AND PROJECT MANAGEMENT
BUILDING G-10, CHAPEL ROAD FORT INDIANTOWN GAP
ANNVILLE, LEBANON COUNTY, PENNSYLVANIA

PROJECT NO. 42240144

FTIG - MUIR AIRFIELD
APRON & PARKING PAD REPAIRS
UNION TOWNSHIP
LEBANON COUNTY, PENNSYLVANIA

MILLING DISPOSAL/ROADS

VERIFY SCALE	DRAWN BY			DATE			DRAWING NO.		
BAR IS ONE (1) INCH LONG ON ORIGINAL DRAWING: 0  1	DAS	17 APR 25		KEL	SCALE	AS NOTED	C.1.1		
IF BAR IS NOT ONE (1) INCH LONG, ADJUST SCALE ACCORDINGLY									

CONTRACTOR SHALL FIELD VERIFY
ALL DIMENSIONS
VARIANCE FROM CONTRACT
DOCUMENTS NOT PERMITTED
WITHOUT BUREAU OF ENGINEERING
AND ARCHITECTURE APPROVAL.