

COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF MILITARY AND VETERANS' AFFAIRS
ANNVILLE, PENNSYLVANIA

JOSH SHAPIRO, GOVERNOR

MAJOR GENERAL JOHN R. PIPPY, THE ADJUTANT GENERAL

PROJECT NO.: 42240144
FORT INDIANTOWN GAP - MUIR AIRFIELD
APRON & PARKING PAD REPAIRS
UNION TWP, LEBANON COUNTY, PENNSYLVANIA

DESIGN PROFESSIONALS:
OFFICE OF FACILITIES AND ENGINEERING
BUREAU OF DESIGN AND PROJECT MANAGEMENT
BUILDING 0-10, CHAPEL ROAD, FORT INDIANTOWN GAP
ANNVILLE, LEBANON COUNTY, PENNSYLVANIA

INDEX TO DRAWINGS

G.1.1 COVER SHEET

GENERAL CONSTRUCTION

CIVIL DWGS.

C.1.0 APRON PLAN AND DETAILS

C.1.1 MILLING DISPOSAL/ROADS

NOTES

GENERAL NOTES

- THE PLAN IS BASED ON AERIAL IMAGERY AND FIELD MEASUREMENTS. A TOPOGRAPHIC SURVEY WAS NOT PERFORMED.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY ACTUAL SITE CONDITIONS PRIOR TO THE START OF ANY WORK. ANY DISCREPANCIES FOUND SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE CONTRACTING OFFICER AND PROJECT MANAGER PRIOR TO THE START OF WORK.
- THE LOCATION OF UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES BEFORE COMMENCING WITH ANY WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING A DIG PERMIT FROM FTIG-DIM. THE CONTRACTOR SHALL ALSO NOTIFY PA-ONE-CALL NOT LESS THAN THREE DAYS AND NO MORE THAN 10 DAYS PRIOR TO THE START OF WORK.
- CONTRACTOR SHALL COORDINATE ALL OPERATIONS WITH AASF CONTROL TOWER.
- THE CONTRACTOR SHALL STAKE OUT THE LOCATION OF ALL NEW CONSTRUCTION AND VERIFY ALL SETBACKS, OFFSETS, AND CLEARANCES PRIOR TO START OF WORK.
- CONTRACTOR TO MAINTAIN POSITIVE DRAINAGE AT ALL TIMES.
- NUMERICAL DIMENSIONS AND FIELD MEASUREMENTS SHALL SUPERCEDE ANY DISCREPANCY IN THE SCALING OF THE DRAWINGS.

PAVING NOTES

- TOTAL AREA OF REPAVING IS APPROXIMATELY 53,000 SY. COLD MILL 2 INCHES OF EXISTING BITUMINOUS MATERIALS.
- DURING MILLING PROCESS, DO NOT DAMAGE CONCRETE PARKING APRONS, CURBS, UTILITIES, OR SURROUNDING SITE FEATURES. CONTRACTOR WILL BE RESPONSIBLE TO REPAIR ANY DAMAGE AT THEIR OWN EXPENSE.
- CONTRACTOR SHALL REMOVE AND RECLAIM ASPHALT MILLINGS FOR USE IN CONSTRUCTION OF PERIMETER ROAD - SHE SHEETS C.1.1 AND C.1.2.
- PROOF ROLL SUB-BASE BEFORE PAVING. A REPRESENTATIVE OF THE DEPARTMENT MUST BE PRESENT DURING THE PROOF ROLL OPERATION.
- PLACE 2-1/2 INCHES OF 19 MM PENNDOT APPROVED SUPER PAVE. FINAL PAVE SHALL MEET EXISTING ELEVATIONS AND GRADES.
- ALL EXISTING FRAMES, GRATES, LIDS, AND COVERS SHALL BE ADJUSTED IF NEEDED TO MEET PROPOSED PAVEMENT ELEVATIONS.
- SEAL AROUND ALL CONCRETE PARKING (MOORING) PADS ENSURING SEALER DOES NOT EXTEND MORE THAN 2 INCHES ALONG THE HORIZONTAL SURFACE OF THE CONCRETE PAD.

SEALING NOTES

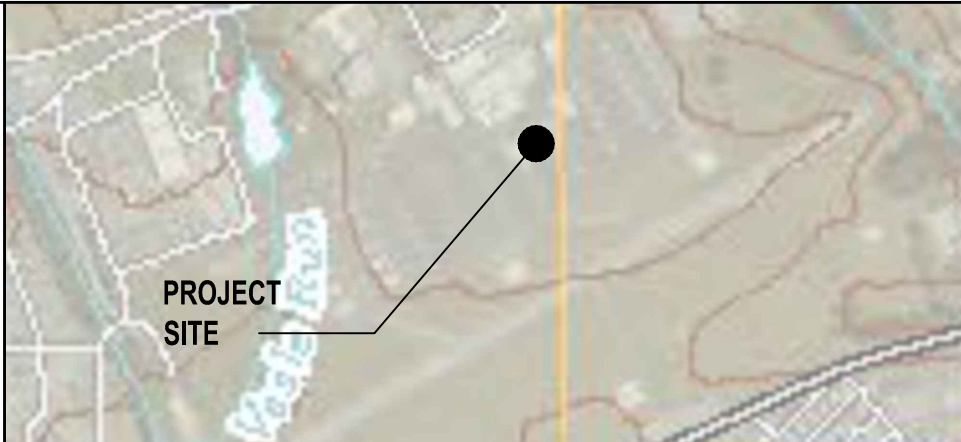
- TOTAL AREA OF SEALING IS APPROXIMATELY 10,000 LF ALONG SEAMS OF PRIOR PAVING. SEALER SHALL NOT EXTEND MORE THAN 2 INCHES ALONG THE HORIZONTAL SURFACE OF THE APRON. REF. PENNDOT 408, SECTION 469.

CONCRETE PARKING (MOORING) PAD NOTES

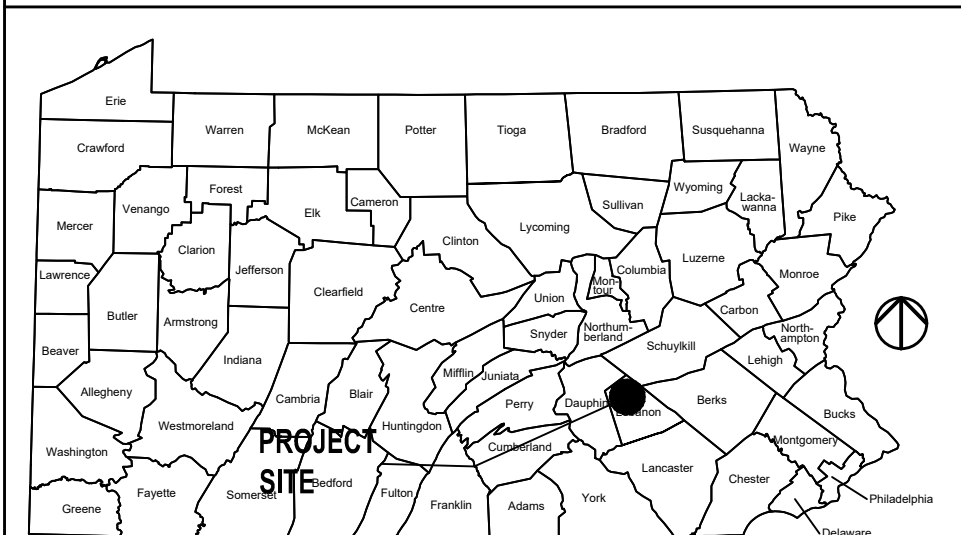
- CONSTRUCT FOUR (4) NEW CONCRETE PARKING PADS AS SHOWN ON THE PLAN AND DETAILS.

PAINTING NOTES

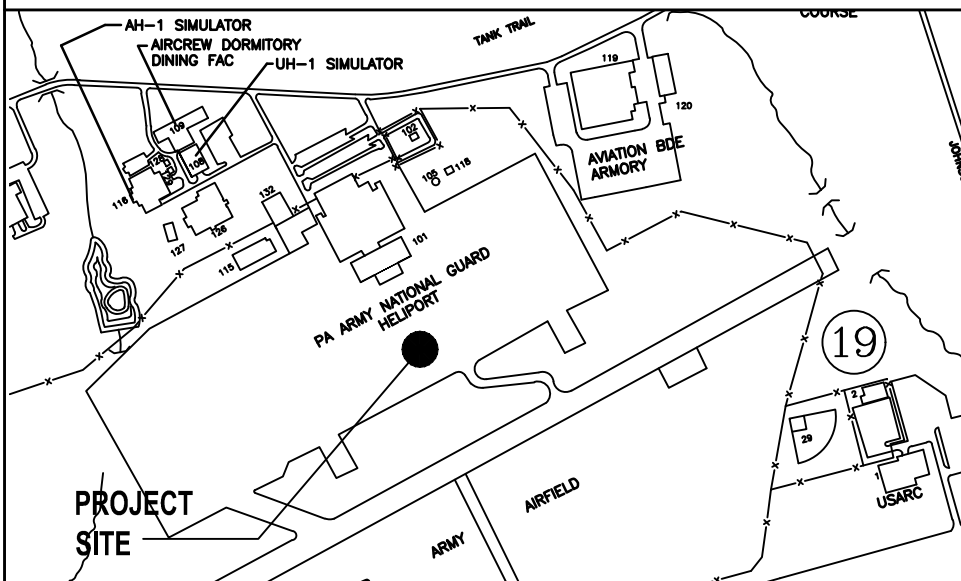
- REPAINT TAXI LINES AND PAVEMENT MARKING ON APRONS PER AASF SPECIFICATIONS.
- PAINT STATE OUTLINE, KEYSTONE, AND TEXT IN NEW LOCATION PER AASF SPECIFICATION AND DETAIL.



PROJECT LOCATION MAP



VICINITY MAP



CAMPUS / KEY PLAN

NO.	DESCRIPTION	DATE

REVISIONS

CALL BEFORE YOU DIG!
PENNSYLVANIA LAW REQUIRES
3 WORKING DAYS NOTICE FOR
CONSTRUCTION PHASE
AND 10 WORKING DAYS IN DESIGN STAGE
STOP CALL
Pennsylvania One Call System, Inc
1-800-242-1776

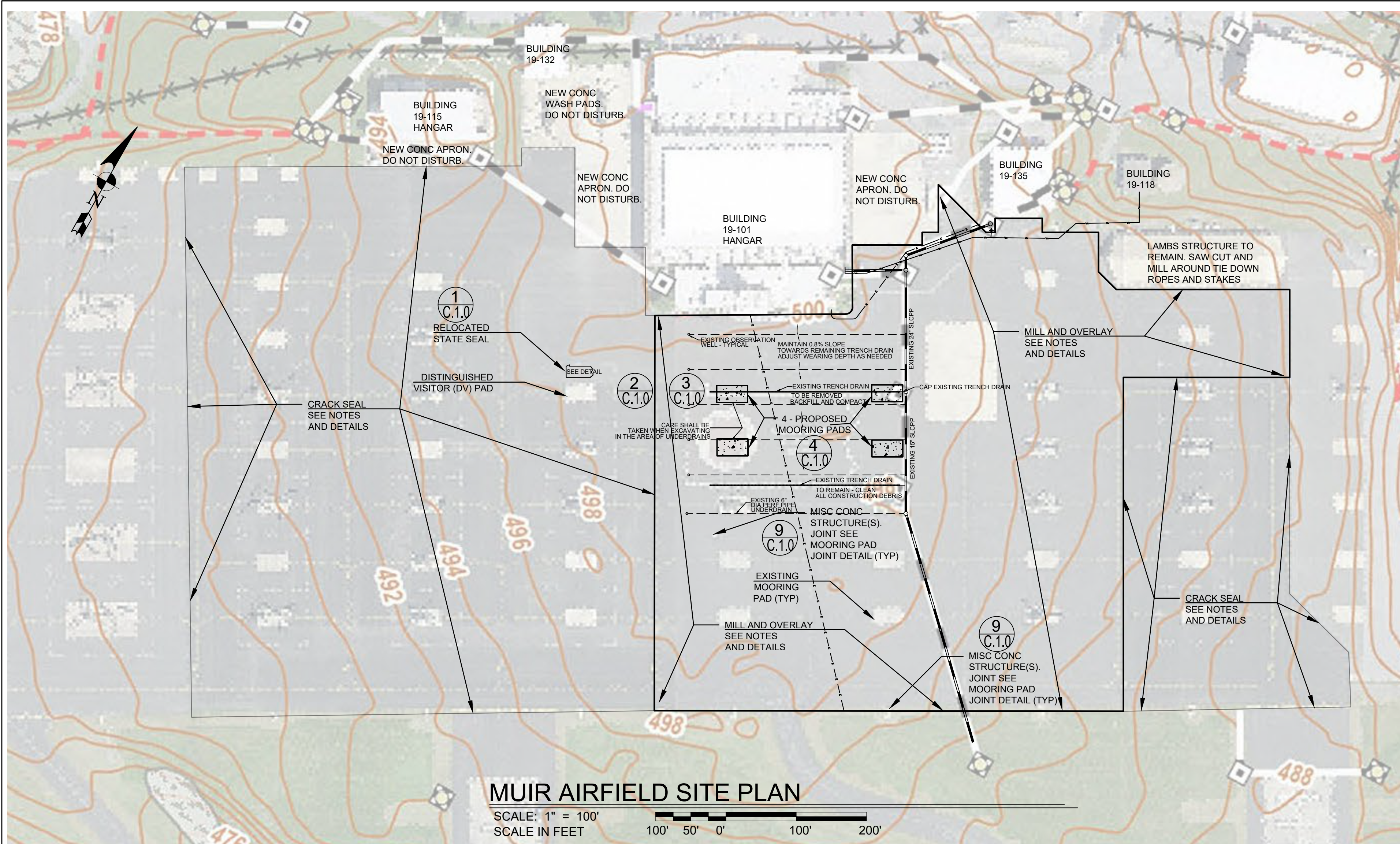
Professional's Signature Date Professional's Signature Date

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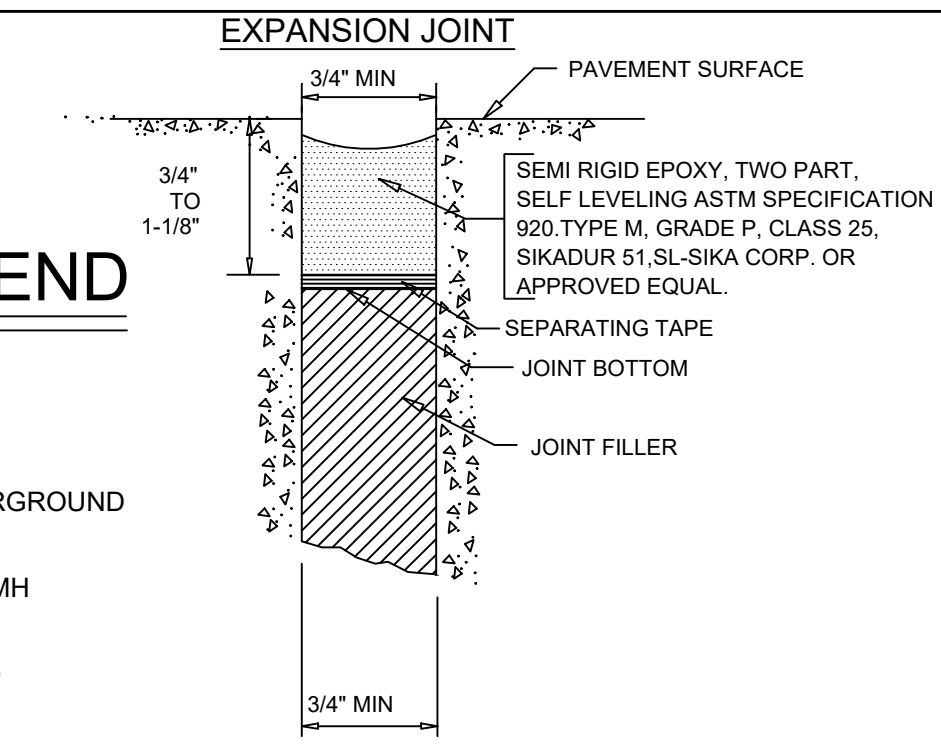
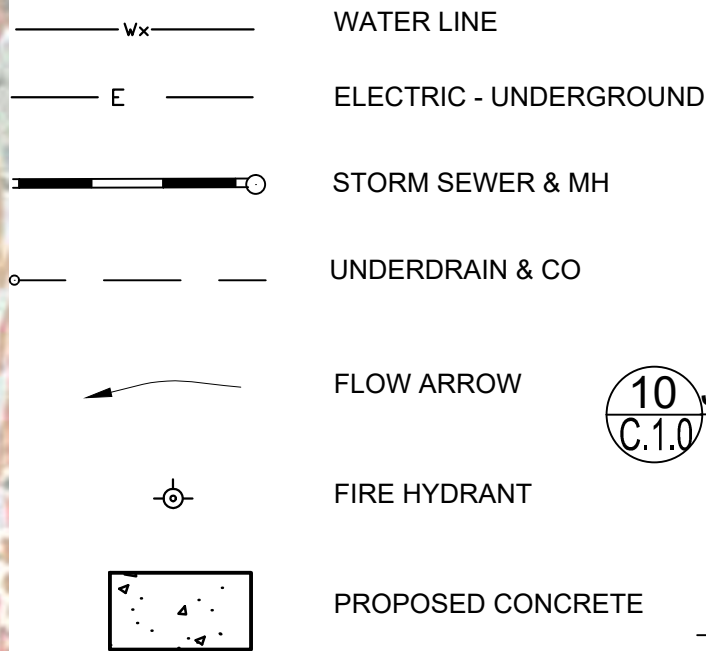
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DMVA PROJECT NO. 42240114
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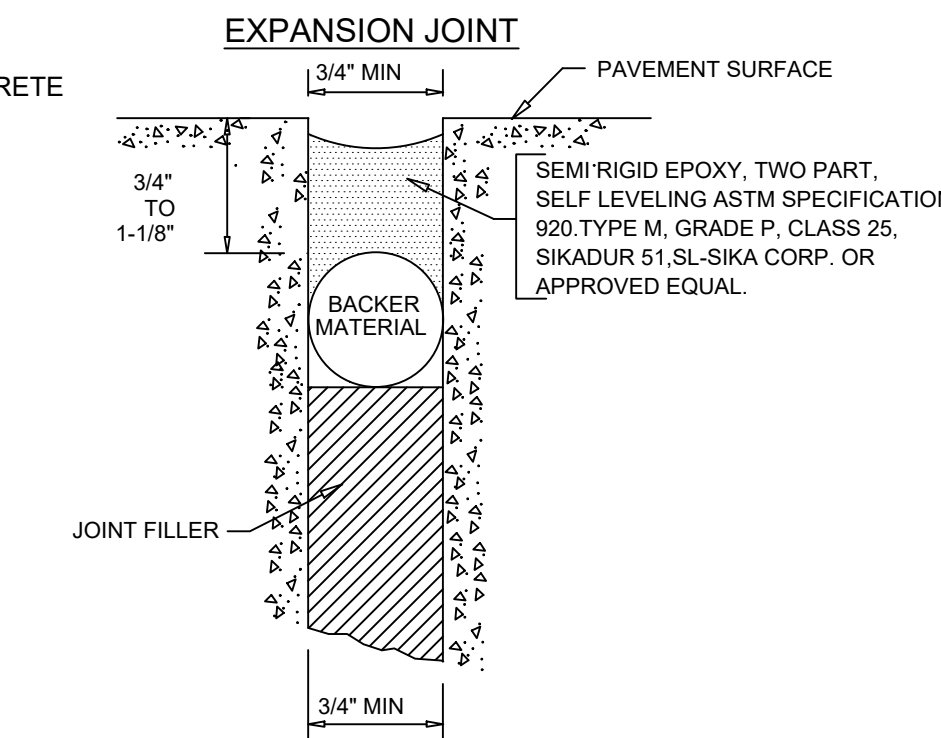
COVER SHEET AND NOTES		
DRAWN BY DAS	DATE 17 APR 25	DRAWING NO. G.1.1
CHECKED BY KEL	SCALE AS NOTED	



SITE / CIVIL LEGEND

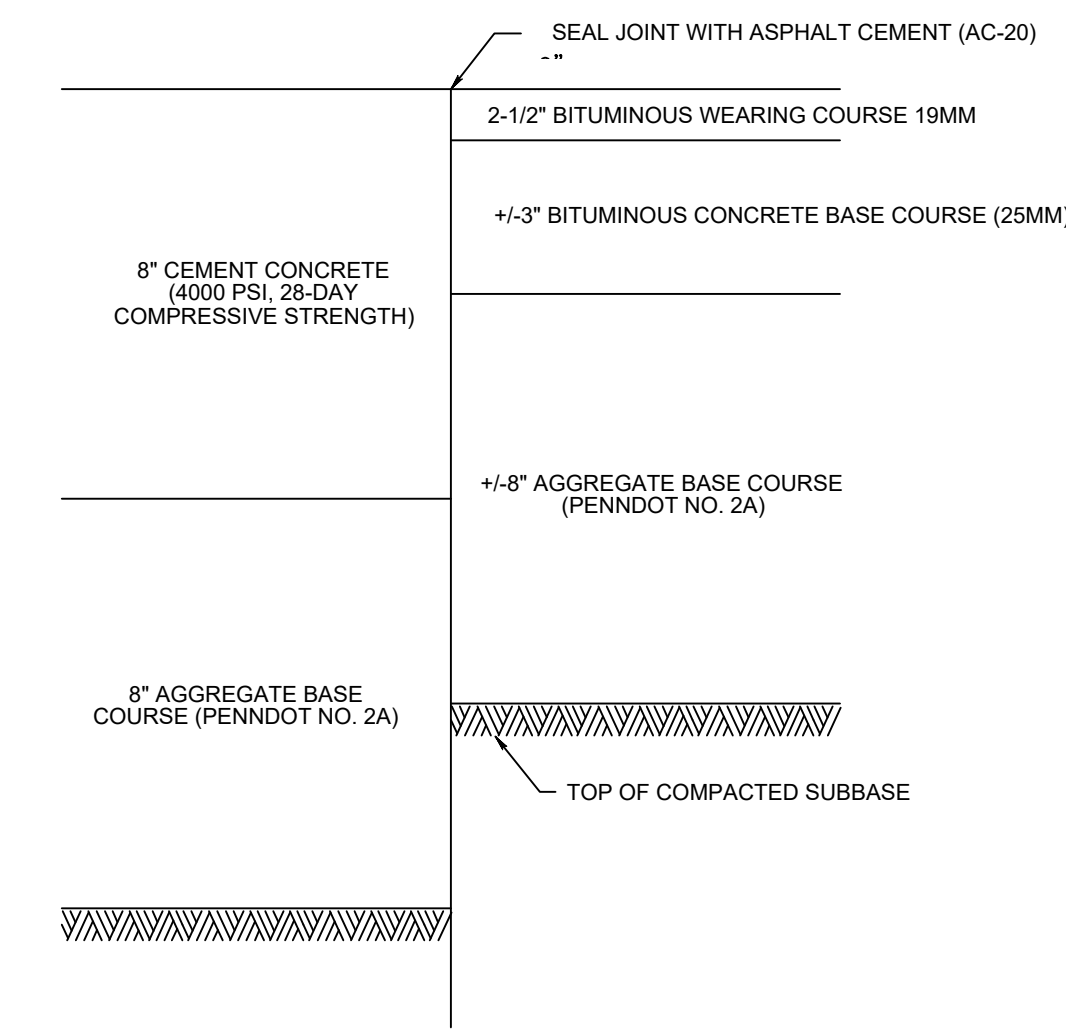


10 JOINT SEALANT DETAILS (SEPARATION TAPE)

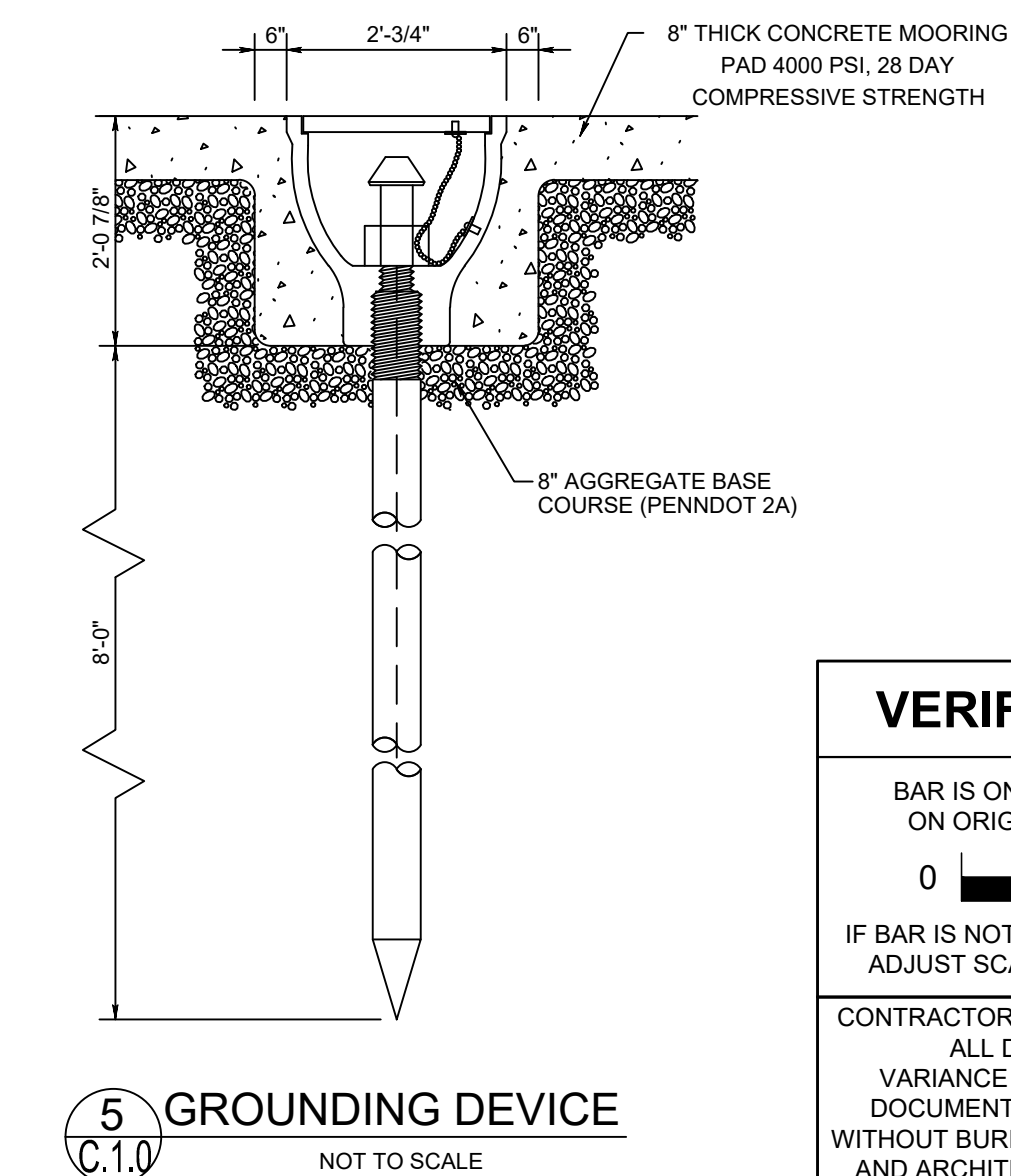


10 JOINT SEALANT DETAILS (BACKER MATERIAL)

NOTE:
USE EITHER BACKER MATERIAL METHOD OR SEPARATION TAPE METHOD FOR SEALING JOINTS.



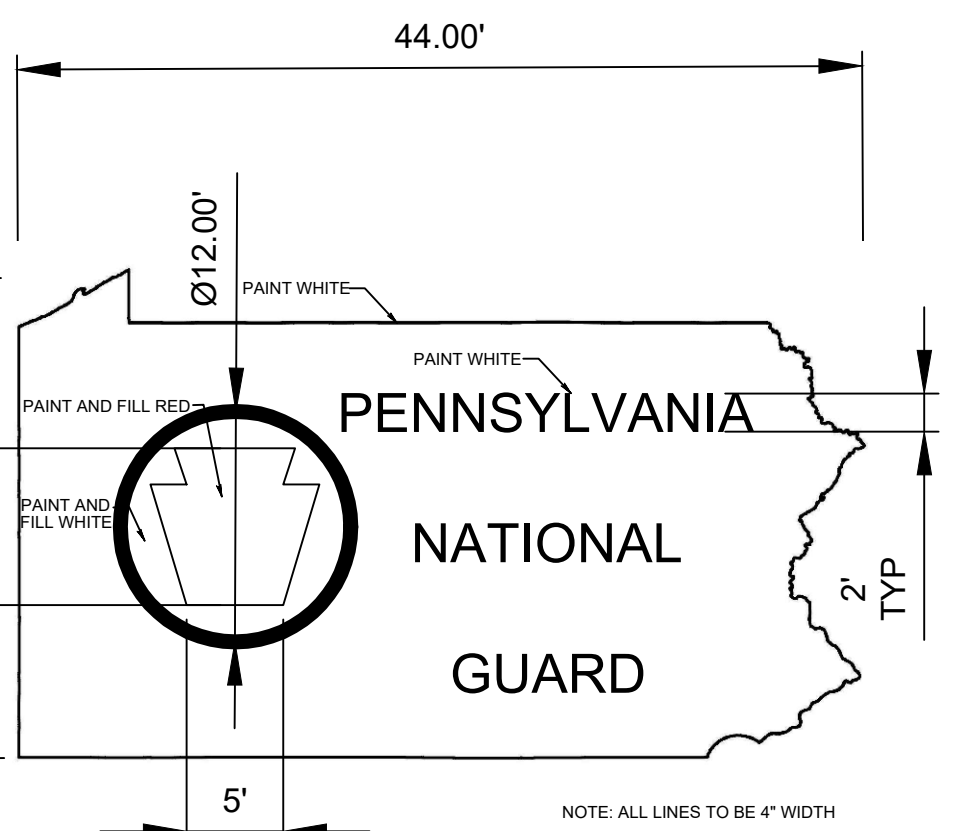
9 JOINT BETWEEN CONCRETE MOORING PAD AND BITUMINOUS PAVEMENT SECTION



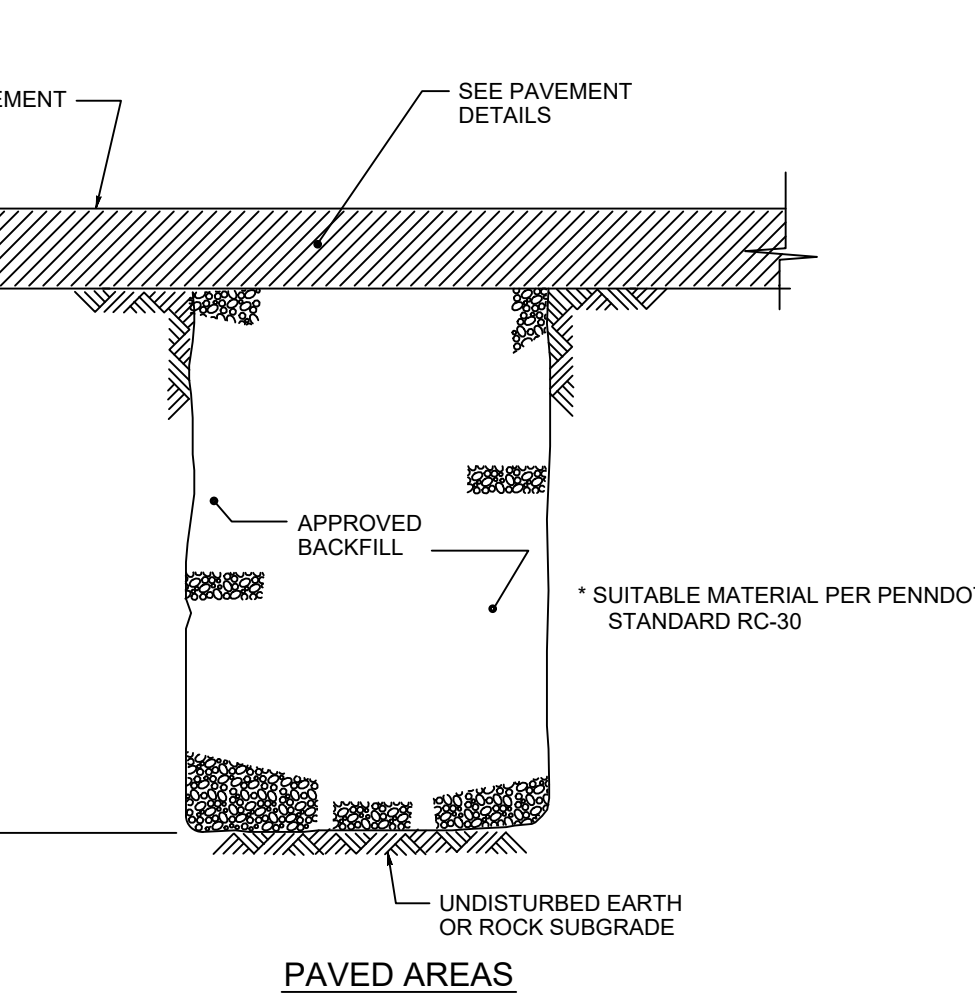
VERIFY SCALE

BAR IS ONE (1) INCH LONG ON ORIGINAL DRAWING:
0 1
IF BAR IS NOT ONE (1) INCH LONG, ADJUST SCALE ACCORDINGLY

CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS VARIANCE FROM CONTRACT DOCUMENTS NOT PERMITTED WITHOUT BUREAU OF ENGINEERING AND ARCHITECTURE APPROVAL.



1 STATE SEAL PAINT DETAIL



2 TRENCH REPAIR DETAIL

NO.	DESCRIPTION	DATE
REVISIONS		

Professional's Signature	Date
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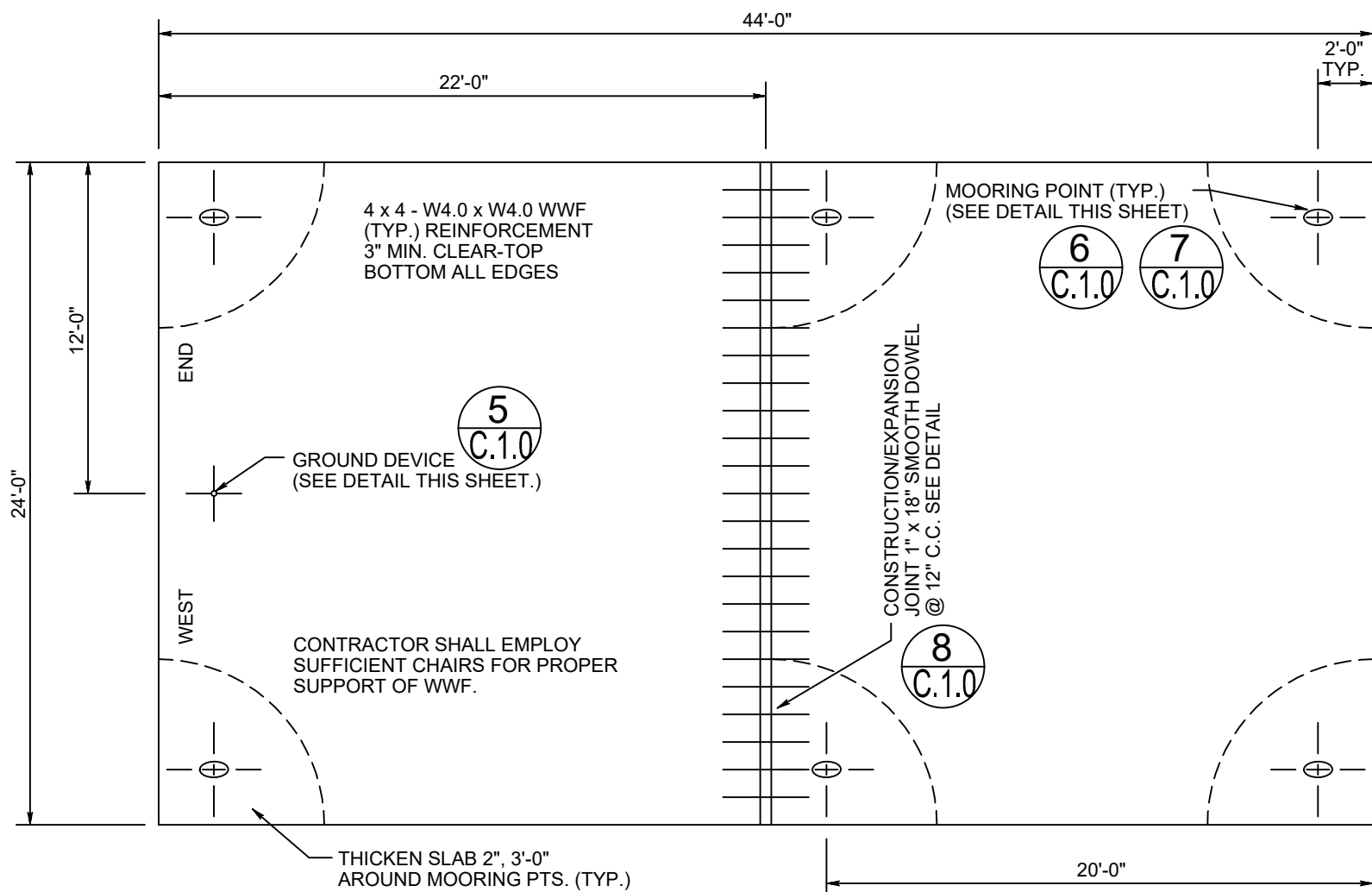
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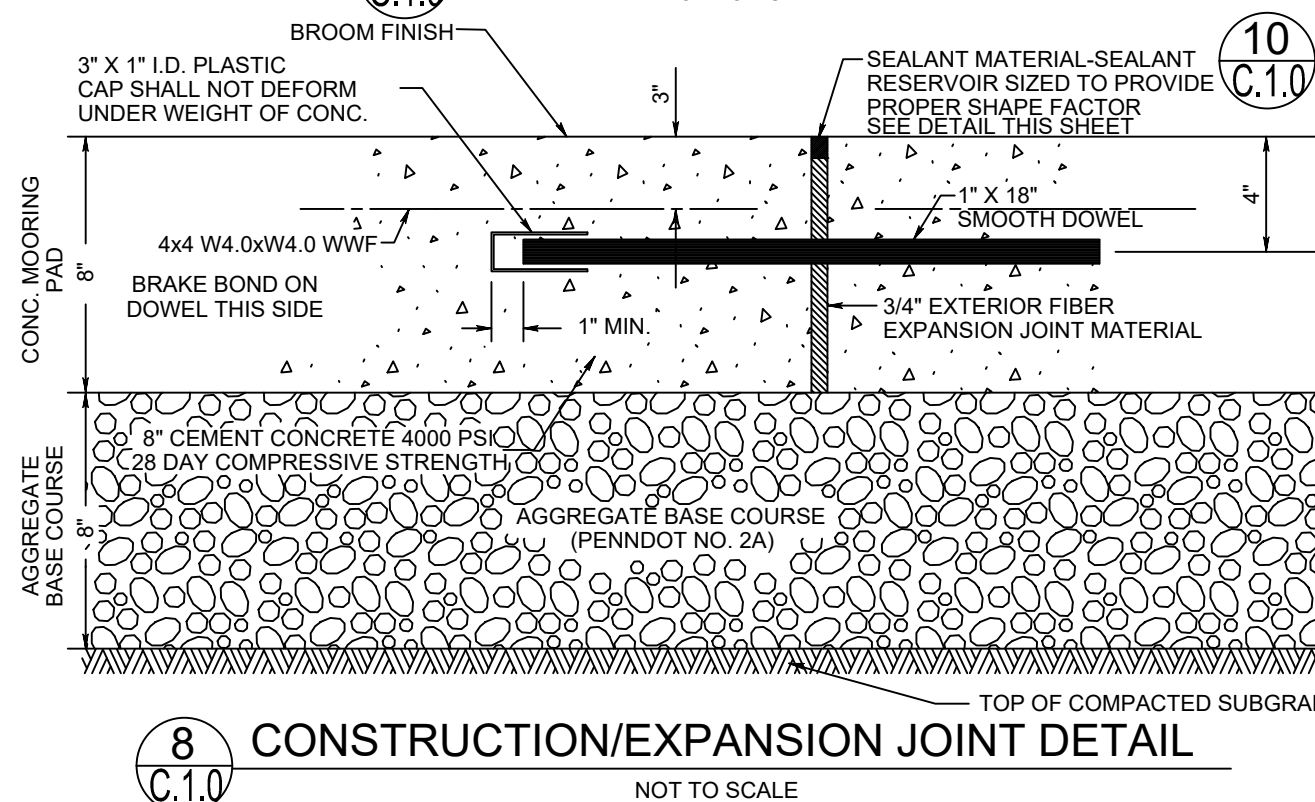
APRON PLAN AND DETAILS

DRAWN BY DAS	DATE 17 APR 25	DRAWING NO. C.1.0
CHECKED BY KEL	SCALE AS NOTED	



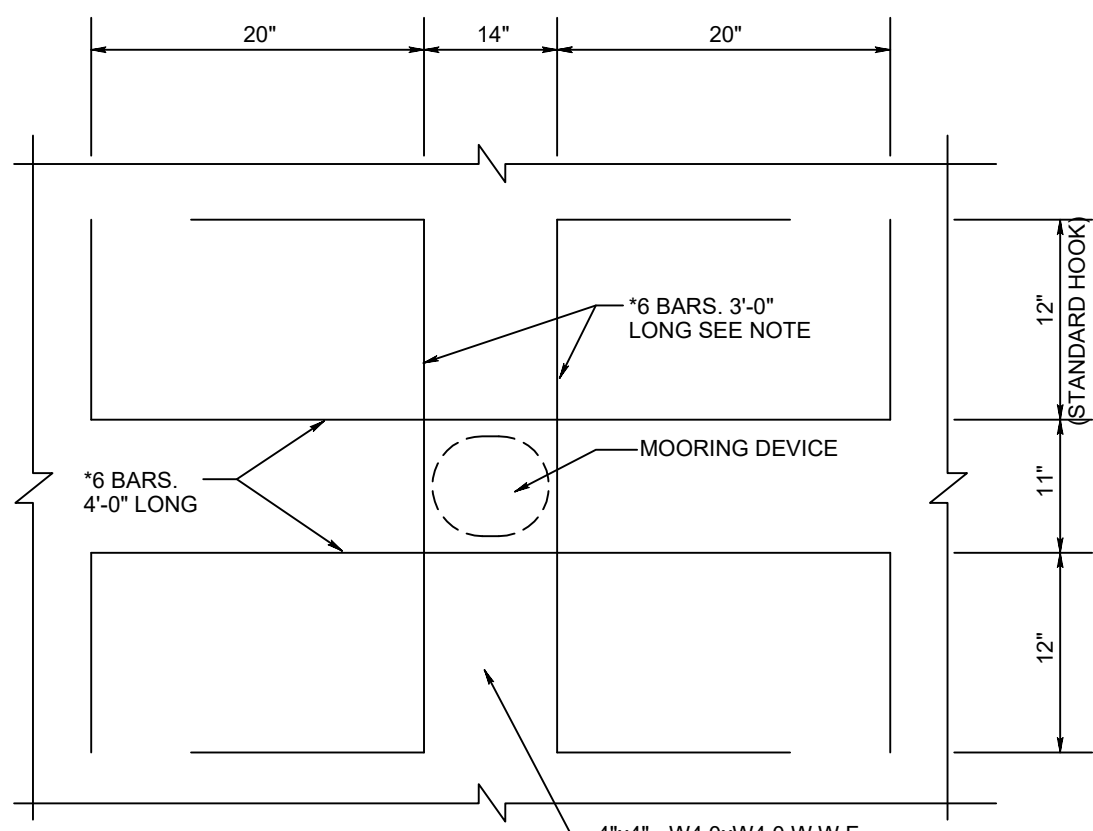
4 CONCRETE MOORING PAD

NOT TO SCALE



8 CONSTRUCTION/EXPANSION JOINT DETAIL

NOT TO SCALE



THE ENDS OF REINFORCING BARS SHOULD BE PLACED 3 INCHES FROM PAVING.

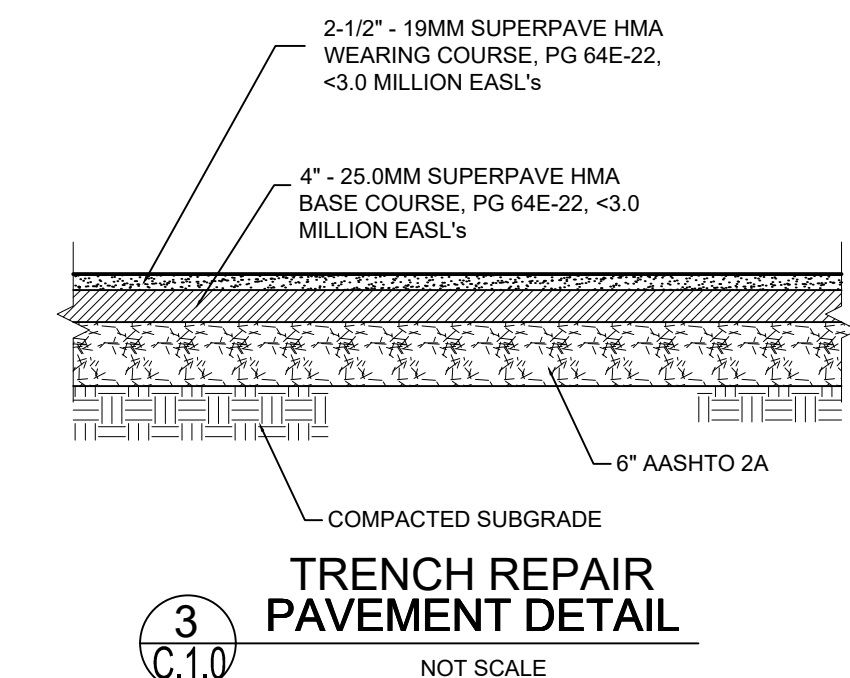
PLAN VIEW

NOTES

- THESE #6 REINFORCING BARS SHOULD BE PLACED 3 INCHES FROM MOORING DEVICE AND 3 INCHES FROM TOP OF PAD.
- THE ENDS OF REINFORCING BARS SHOULD BE PLACED 3 INCHES FROM PAVING JOINTS TO PROVIDE COVER.

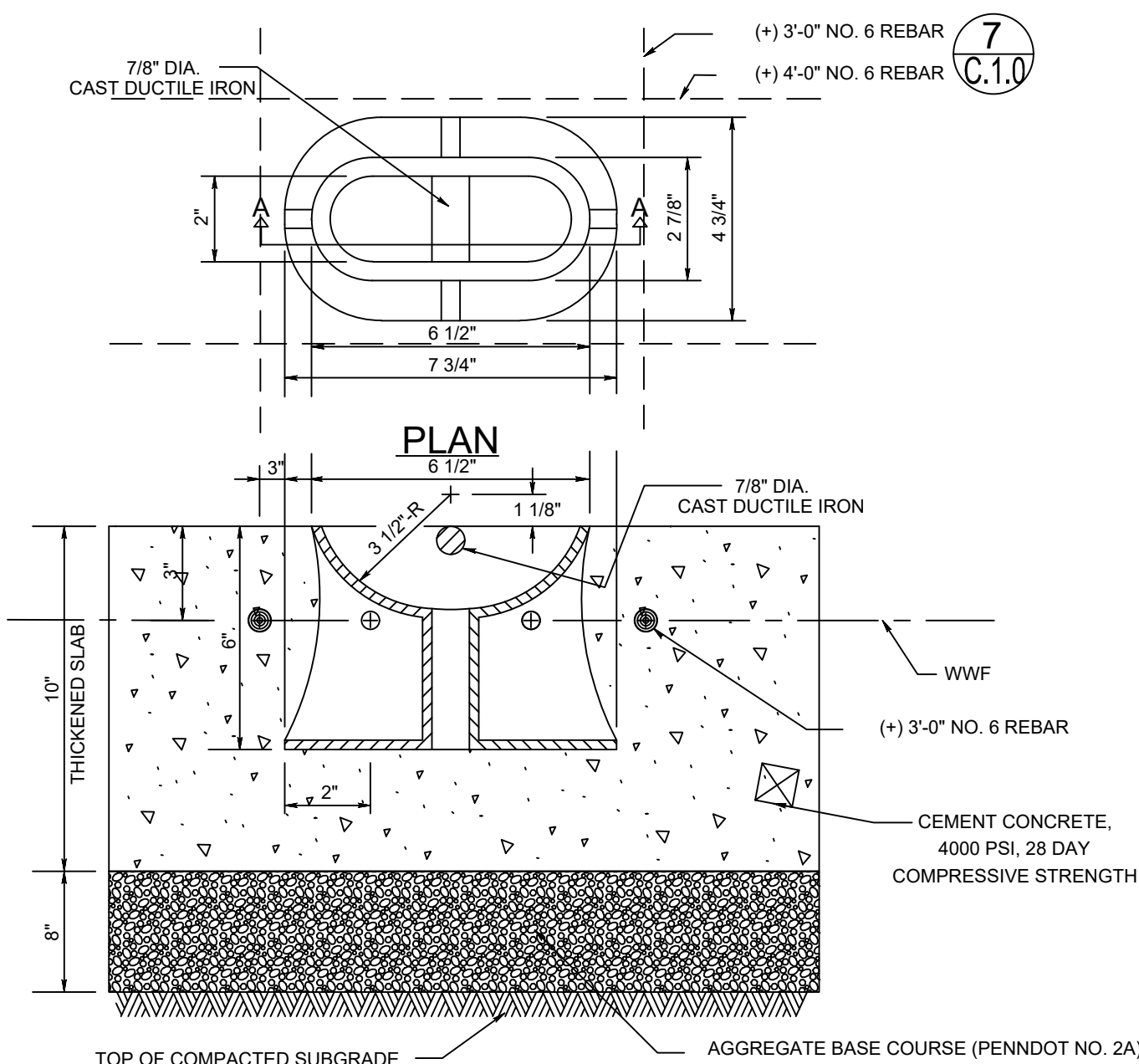
7 MOORING DEVICE REINFORCEMENT

NOT TO SCALE



3 TRENCH REPAIR PAVEMENT DETAIL

NOT SCALE



SECTION A-A MOORING POINT

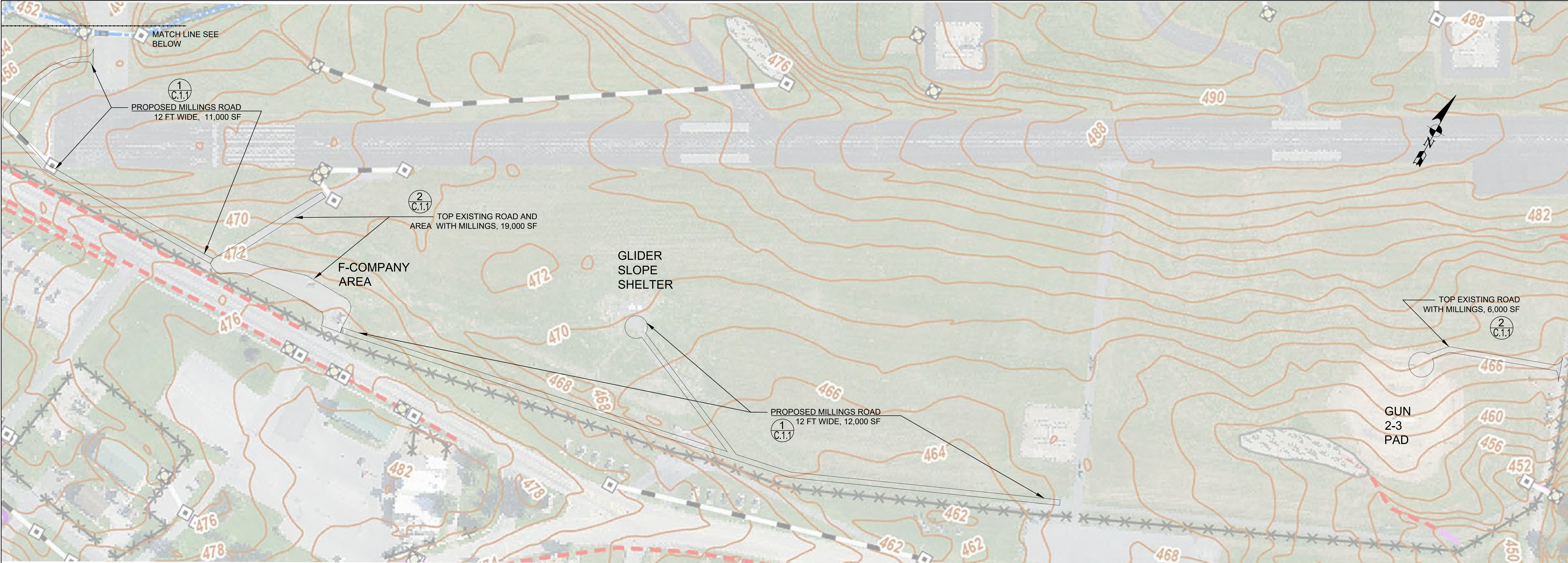
NOT TO SCALE

NOTE

- MOORING DEVICE TO BE CAST IN DUCTILE IRON 80-55-06.
- WELDED WIRE FABRIC MAY HAVE TO BE CUT FOR ANCHOR INSTALLATION REBAR SHALL BE INSTALLED BELOW WWF.
- NEENAH AIRCRAFT MOORING EYES R-3490-A OR APPROVED EQUAL.
- THE ENDS OF REINFORCING BARS SHALL BE PLACED 3" FROM PAVING JOINTS TO BE PROVIDE COVER.
- THICKEN SLAB AN ADDITIONAL 2" FOR 3'-0" AROUND MOORING POINTS.

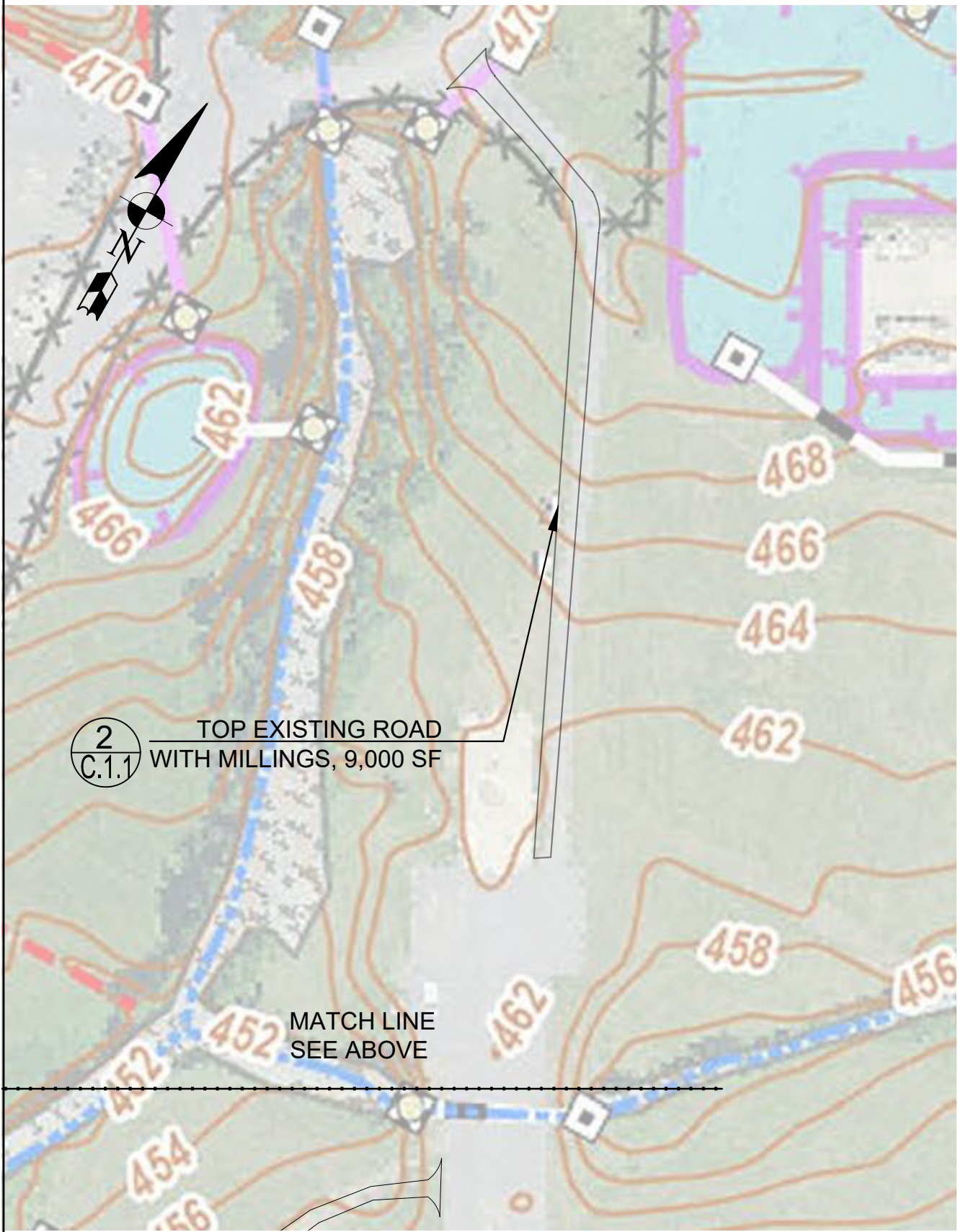
5 GROUNDING DEVICE

NOT TO SCALE



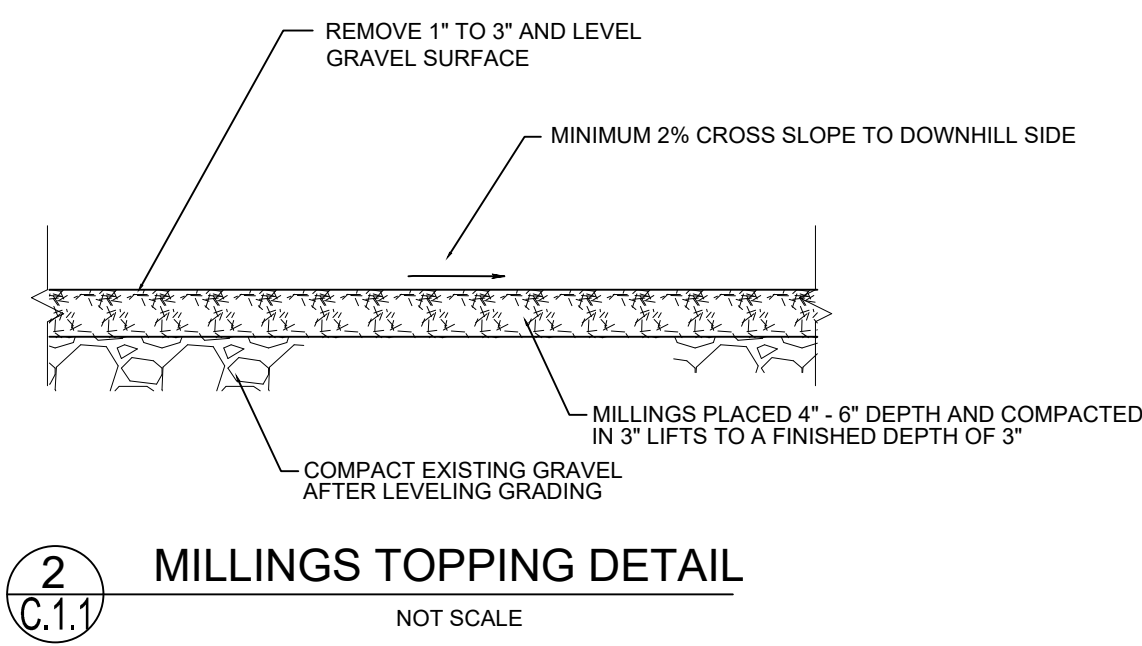
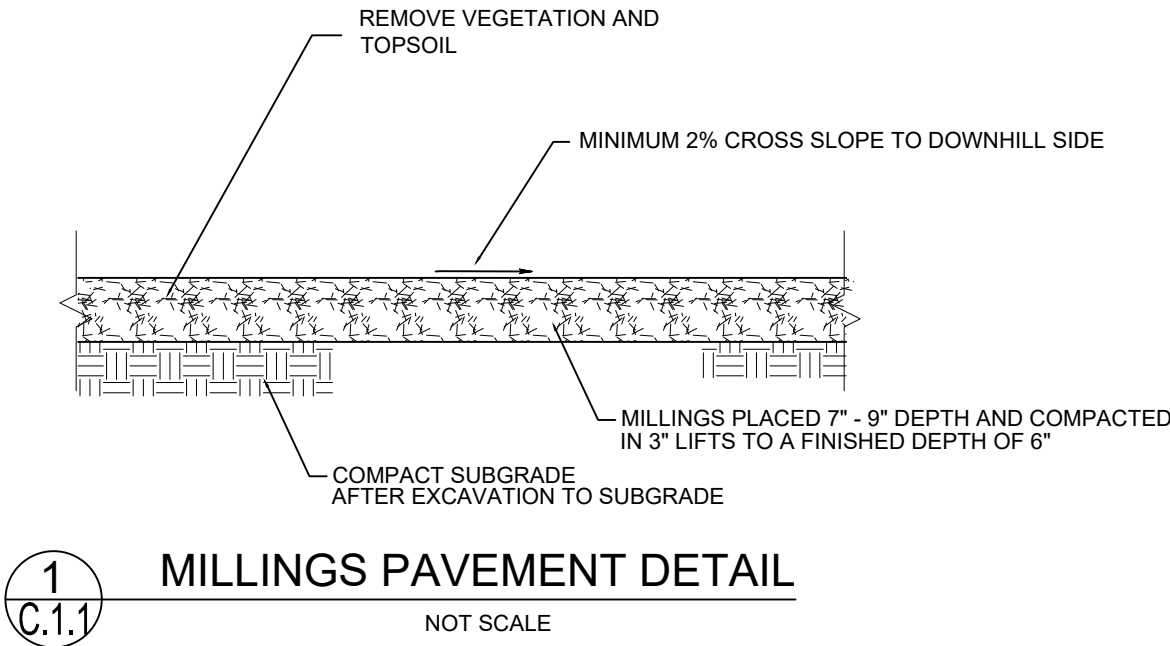
MUIR AIRFIELD ROADWAYS

SCALE: 1" = 100'
SCALE IN FEET



MILLING NOTES

- MILLINGS SHALL BE FINE GROUND TO UNIFORM DIAMETER TO BE USED FOR PAVING NEW ROAD.
- REMOVE LOOSE ASPHALT, DEBRIS, VEGETATION, AND TOPSOIL IN AREAS OF PROPOSED MILLINGS SURFACE TO A DEPTH OF 4-6 INCHES. DISPOSE OF LOOSE ASPHALT AND DEBRIS IN DUMPSTER AND VEGETATION AND TOPSOIL IN DESIGNATED AREAS.
- GRADE AND COMPACT SURFACE FOR DRAINAGE AND SOLID BASE.
- FOR NEW ROADS, SPREAD MILLINGS EVENLY TO A LOOSE DEPTH OF 7-9 INCHES. ROLL TO COMPACT AFTER EVERY THREE INCHES TO 6 INCHES AFTER COMPACTION.
- FOR TOPPING EXISTING ROADS, SPREAD MILLINGS EVENLY TO A LOOSE DEPTH OF 4-5 INCHES. ROLL TO COMPACT TO 3 INCHES AFTER COMPACTION.



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Professional's Signature _____ Date _____

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